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VEHICLE AND PEDESTRIAN MANAGEMENT PLAN

BERRIMA CEMENT WORKS

&

BLUE SHALE QUARRY



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1. Purpose

The Vehicle and Pedestrian Management Plan (this Plan) has been designed for the Berrima Cement Works and Blue Shale Quarry under normal operating conditions. For extraordinary maintenance or shut down works, an additional plan will be created to capture temporary changes to the vehicle and pedestrian interfaces.

1.1 Blue Shale Quarry Requirements

How the requirements of the WHS Mining Regulations are considered and met are documented in the Mine Safety Management System document.

2. Roles and Responsibilities

Site Operations Manager	Responsible for ensuring: <ul style="list-style-type: none"> • Establishment of this Plan • performance objectives and targets are established, monitored and achieved • availability of resources • Communicating the importance of this Plan, meeting statutory and regulatory requirements; • Conducting management reviews of this Plan every 2 years • Verifying the implementation of corrective actions • Recognising and responding to community concerns.
Production Manager / Technical Manager / Engineering Manager	<ul style="list-style-type: none"> • Ensuring staff within their area of responsibility are trained and updated on this Plan, responsibilities, instructions and procedures; • Reviewing traffic operations and implementing strategies to minimise the potential for traffic conflict and/or personal injury resulting from traffic movement; • Coordinating the auditing of the Plan to ensure compliance • Ensuring traffic and pedestrian incidents are investigated and corrective action taken; • Ensuring all training is recorded and monitored
Team Leaders / Front Line Supervisors	<ul style="list-style-type: none"> • Identifying, reducing and preventing traffic movements that may result in traffic conflict and/or personal injury; • Reporting non-compliances with this plan • Initiating actions to prevent traffic and pedestrian incidents; • Reporting and recording traffic and pedestrian incidents; • Initiating corrective actions to repair harm caused by traffic incidents.
WHS Business Partner	<ul style="list-style-type: none"> • Assisting the site to prepare and review this Plan. • Checking the implementation of this Plan



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Employees, Contractors and Visitors	<ul style="list-style-type: none">• Obeying all traffic and pedestrian signage and speed limits within the site• Complying with any traffic management instructions and procedures that apply to their work or operations;• Reporting traffic incidents;
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3. Health, Safety and Environment Considerations

3.1 Noise

Truck movement hours are not restricted. To minimise the impact of truck movements on the local community the majority of despatches are made on working week days between 0600 and 1800 hours.

The majority of incoming trucks with delivery of alternative fuels such as trye and wood chips also occur during similar hours.

3.2 Dust

All raw materials, including alternative fuels and finished products being carted to and from the site by road shall be covered to minimise dust emissions.

All vehicles exiting the Quarry and the Shale Pad are to exit via the wheel wash stations to minimise drag out from the areas.

Dust suppression will be performed as required.

3.3 Training

Training on this Plan includes toolbox talks, displaying of maps and site inductions.

3.4 Site Access

Access to the site is from Taylor Avenue via two security gates, Gate 1 “Argyle Street Entrance” and Gate 2 “Perth Street Entrance”. These two entrances are adjacent to the village of New Berrima.

Incoming heavy vehicles (Tippers, Tankers, B-doubles etc) are expected to use the Perth Street entrance. It may be necessary to allow heavy vehicle to use the Argyle Street entrance under strict conditions, for example, in periods of high movement volume or Perth Street entrance closure.

3.5 General Traffic Rules

As a minimum NSW Transport for NSW Road Rules and any SafeWork NSW licencing requirements apply on site. Additionally:

- Do not stand within 25m of tipping trucks (or 1½ times the tipping body length)
- Pedestrians to give way to vehicles and mobile plant



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- Three points of contact is to be used while climbing on or off any piece of equipment or when using stairs. If the access is near vertical, face the ladder or catwalk when climbing on and off the item of equipment, and never jump off machinery.
- The site has a Zero Breath Alcohol policy. The Boral Cement Alcohol and Other Drugs procedure applies to all persons at Berrima Cement Works.

3.6 Signage, Markings and Separation

3.6.1 Signage

Roads, traffic ways, pedestrian traffic ways and parking areas are to be clearly sign posted to define the requirements.

3.6.2 Markings

- Pedestrian crossing and walkways are to be clearly identified
- Parking bays are to have clear delineation

3.6.3 Separation

- Areas noted in the risk assessment are to have separation between vehicles/mobile plant and pedestrians
- No person is permitted to pass through a danger barricade without seeking approval from the name on the tag. Pedestrians can move through caution barricades after reading tag details.
- No person is permitted to pass/drive over the peak of a windrow/bund.
- Appointed traffic controllers will control vehicle interactions when required.

3.7 Speed Limits

The speeds shown below are the maximum allowable speed, however operators must drive to the prevailing conditions. Be wary of poor visibility, crowded areas and poor road surfaces.

The following MAXIMUM speed limits apply on the Cement Works site:

- General site speed limits unless signposted 50 km/h;
- Congested or restricted areas and Blue Shale Quarry 20 km/h;
- Shared Zones 10 km/h.

3.8 Accessing the Rail Corridor

Vehicles shall cross the rail corridor at designated crossings. Trains have right of way at all crossings.

No vehicle shall access the rail corridor and travel along the rail line unless;

- The rail line is isolated, AND;



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- The rail infrastructure is concreted to protect it from damage.

3.9 Quarry and Shale Pad

Light vehicles (3T and under) must not enter the quarry area without:

- An operator who is aware of the Cement Works environments and the content of this Plan
- A clearly visible orange flashing light on at all times (no strobe lights)
- A two way radio either installed or hand-held. Use channels 1 or 3. When using the two-way radios hold down the button for a full second prior to speaking to ensure your full message is transmitted.
- Having contacted the machine operator's on the radio stating their intention to enter the quarry area.
- When there is a need to come within 50m of heavy mobile equipment, a response must be obtained from the Operator before proceeding, and the equipment has been powered down and parked in a safe manner;
- The Central Control Room must give permission before a person may access the Shale Pad. Prior to accessing the area, contact must be made with the operators on the Shale Pad and access shall only be made once all operations have ceased. Once granted, pedestrian access to the Shale Pad is only permitted either by car at the western entrance to the Shale Pad, or on foot via the stairs next to the Shale Crusher building. Operations must not recommence until pedestrians have left the Shale Pad.

3.10 Workshop Management

The following rules apply in and around the workshop;

- Entry is via the south facing roller door, exit via the north facing roller door;
- In the confines of the Maintenance Workshop, all heavy mobile equipment will have the right of way at all times.
- The use of horn signals prior to entering or moving to warn pedestrians of the intent to enter is mandatory
- Reversing should be minimised where possible. A spotter must direct all heavy mobile equipment when reversing in and out of the workshop. Barricading should be used to prevent inadvertent access when guiding in heavy mobile equipment;
- Buckets of heavy mobile equipment must be emptied before entering the workshop;
- If the body or bucket of a vehicle is raised, the body or bucket must be empty of material outside before it is raised and safety pins installed and or props used to prevent unwanted lowering of the body or bucket.

3.11 Tyre Chip Storage Area (TCSA)

A front end loader operates in this area along with road trucks delivering tyre chips.



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Section 3.16 outlines the operations for truck movements within the area.
Appendix 5 and 6 identify front end loader and bulk materials traffic routes.

Light vehicles (3T and under) must not enter the TCSA without:

- An operator who is aware of the Cement Works environments and the content of this Plan
- A clearly visible orange flashing light on at all times (no strobe lights)
- A two way radio either installed or hand-held. Use channels 1 or 3. When using the two way radios hold down the button for a full second prior to speaking to ensure your full message is transmitted.
- Having contacted the machine operator's on the radio stating their intention to enter the TCSA area.
- When there is a need to come within 50m of heavy mobile equipment, a response must be obtained from the Operator before proceeding, and the equipment has been powered down and parked in a safe manner;

3.12 Safe Operating / Approach Distance

- The safe distance for following behind an empty or loaded heavy vehicle is 50m;
- The safe approach distance to approaching a loading, dozing or grading unit is 50m before making radio contact. Positive contact is defined as an understandable radio reply and/or a visual gesture confirming permission to approach the machine.

3.13 Parking

Any vehicle or mobile plant parked for the purposes of maintenance should have the wheels chocked prior to work commencing.

Light Vehicles requirements

- Only park in designated areas. No staff parking in visitor parking area unless permission from Manager. Night shift workers are excluded for security reasons.
- Park so the first movement is forward
- Turn off, out in gear and lock vehicle before exiting or when leaving it on site overnight.
- Do not park closer than 50m to operating equipment unless radio contact has been made with the operator, and permission has been given to approach
- Never park immediately in front of or behind Heavy Mobile Equipment without prior positive contact – it is always safest to park beside and or in full view of the equipment driver.

Mobile Equipment/Heavy Vehicles require:

- Park in their designated parking bays where provided;



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- When parking temporarily, where possible reverse into a safe location that allows maximum visibility when moving off;
When parked as a result of a break down, chock wheels.

3.14 Horn Signals

Before starting or moving heavy mobile equipment you must use the correct horn signals to inform other people in the area know what you are going to do. This applies in all areas including the workshop.

The horn signals are to be used when coming around corners and building and when there is no full visibility.

3.15 Stockpile Access

Any tipping / dumping is to be carried out in accordance with the site dumping procedures.

Tipping of material on a stockpile is not permitted if it is being reclaimed from below. All active tip heads or tip areas shall be demarcated as closed below to prevent vehicles from becoming exposed to falling rocks.

Prior to tipping on a new tip-head, a windrow (safety bund) must be established and the Production Services and Logistics Supervisor must inspect the stockpile.

3.16 Deliveries, Loading and Unloading

- All vehicles are to be loaded in such a manner so as to minimise spillage and the load is to be safely distributed. No vehicle is at any time is to be overloaded.
- Trucks must always reverse into the loading position under a loaded bucket. The vehicle must not be operated in such a way that the cab will pass under a loading machine's bucket at any time.
- Trucks delivering trye and wood chips must always reverse into the designated unloading bays. Truck bodies are not to be lifted until the vehicle is stationary. Trucks must not unload material if truck wheels are located on stockpiled material. Trucks must be level.
- Truck drivers may only leave the cab of their vehicle during the loading and/or unloading process following positive communication with the person loading and /or unloading the truck.
- Deliveries are to be undertaken in areas noted on the site map
- The delivery area is to be segregated during loading and unloading using tape barricades etc to prevent pedestrians from entering the area.
- Only authorised people are to load and unload vehicles.
- Drivers of trucks being loaded or unloaded by forklift need to communicate with the forklift operator and establish protocols to ensure the vehicle is not able to move



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unexpectedly and the driver is positioned in a safe location. The driver of the vehicle shall either remain in the cab and the keys given to the forklift operator or driver to exit the cab with his keys and position himself in an agreed location until un/loading is complete.

3.17 Auditing

The Traffic Management Plan Assessment checklist is designed as the audit tool for determining needs, reviews and compliance to this plan.

3.18 Incident and Near Miss Management

All incidents and near misses shall be managed as per the requirements of the Incident Reporting, Investigation and Action Management procedure. In the event of a breach of this Plan, the potential causes of the breach will be investigated and appropriate corrective actions undertaken.

3.19 Emergency Response

All emergencies shall be managed as per the Berrima Cement Works Emergency Response Plan.

4. Associated Documents

- GRP-HSEQ-5-04 Mobile Plant
- GRP-HSEQ-5-05 Light Vehicle Operation
- GRP-HSEQ-5-06 Heavy Vehicle Operation
- GRP-HSEQ07-09 Pedestrian and Vehicle Management
- GRP-HSEQ-7-07 Workplace Signs
- HSEQ-7-09-F02 Traffic Management Risk Assessment;
- HSEQ-7-09-F03 Traffic Management Plan Assessment Checklist;
- HSEQ-7-07-F01 Workplace Signage Inspection Checklist
- Berrima Drawing 40340-8-LOADER TRAFFIC;
- Berrima Drawing 40340s9r-BULK TRUCKS



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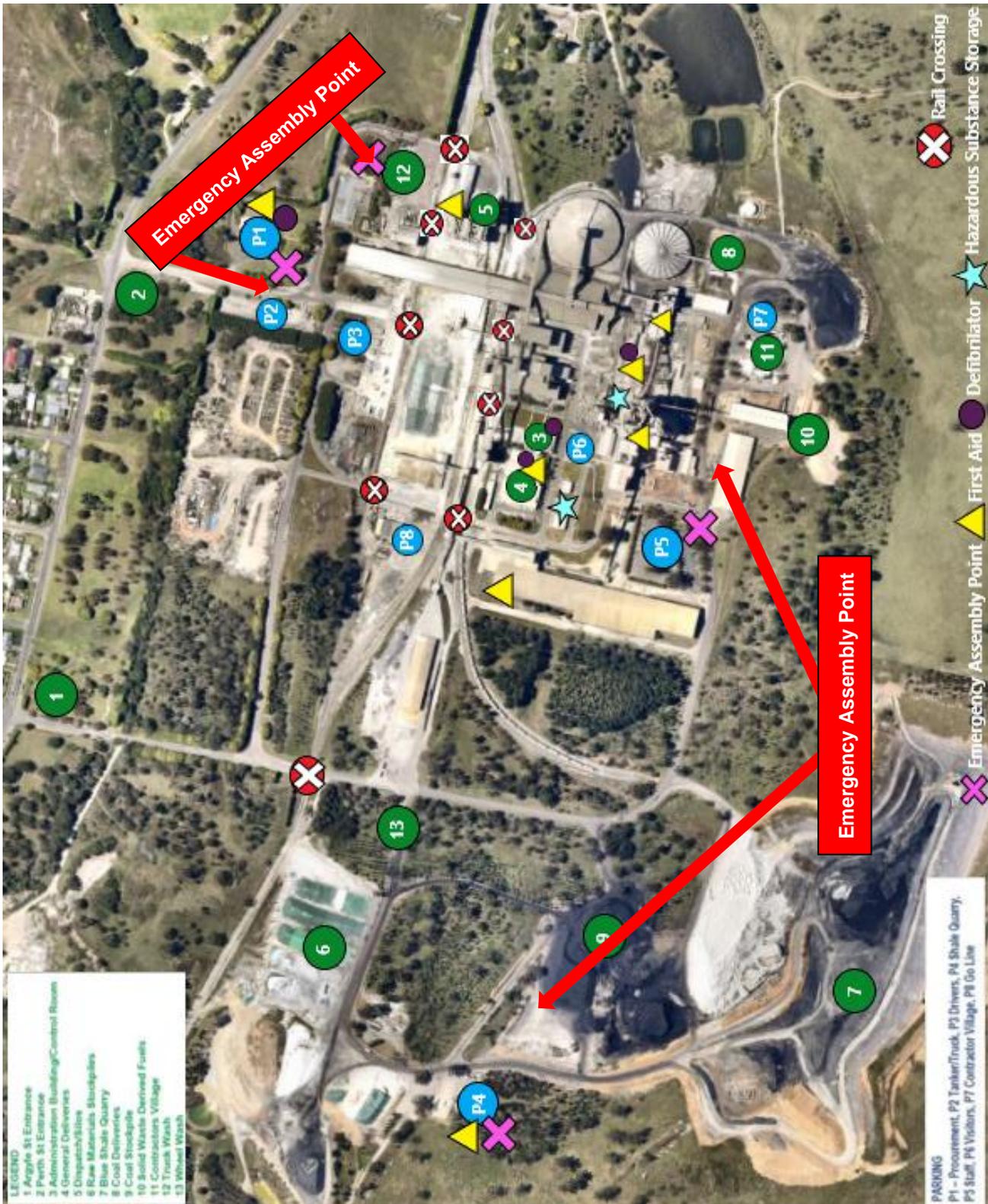
5. Revision History

Version	Date	By Whom	Description of Changes
1	November 2007	Grant Williams	Original version
2	September 2011	Alex Wnorowski	Global revision and formatting change
3	September 2014	Michael Curley	3-yearly review
4	August 2017	Michael Curley	Major revision and formatting changes
5	November 2019	Rob Lasker	Review to align with HSEQ MS
6	September 2020	Naomi Munce	Update and HSR Review
7	June 2023	Jessica Seifert / Sharon Makin	Review and update in response to Mod 15 approval for Tyre Chip storage, removal of intermodal as it no longer operates



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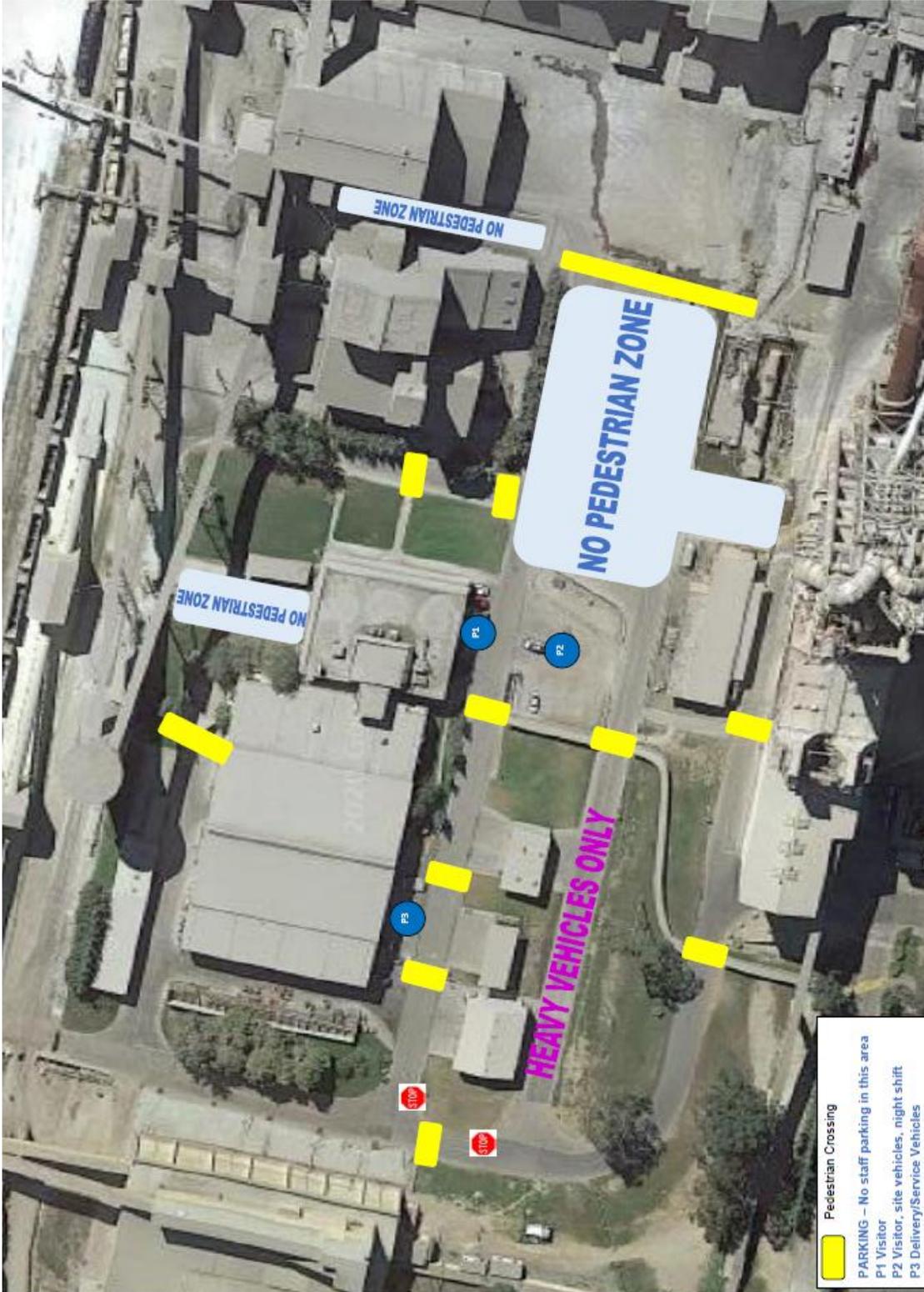
APPENDIX 1 GENERAL SITE GUIDE





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APPENDIX 2 PLANT CENTRE TRAFFIC FLOW





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APPENDIX 3 INTERMODAL TRAFFIC FLOW





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APPENDIX 4 BLUE SHALE QUARRY TRAFFIC FLOW & EMERGENCY RESPONSE

